



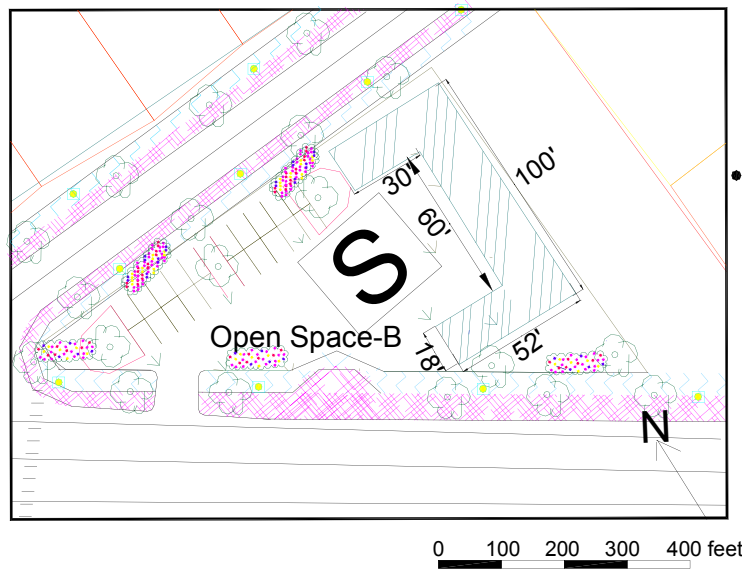
LAMAR CORRIDOR TRANSIT ORIENTED REVITALIZATION PLAN

MEMPHIS, TENNESSEE

PREPARED BY:
PLAN 7008: SITE PLANNING
FALL 2002

Cooper/Parkway

Lamar and Carnes Light Rail Stops with openspace



- *Light rail stop with plaza like environment with parking & open space for pedestrian congregation*

Road
Transit Stop



Telephone
Newspaper



Trash Receptacle



New footprints



Bicycle Path



Pedestrian Path



Landscaping



Street Lights



Open Space-B

Site Planning, Fall 2002

ORANGE MOUND -

EXISTING CONDITIONS

The Orange Mound site is located at the intersection of Lamar and Kerr. This site is bounded on all sides by commercial in a major commercial center near the intersection of Lamar and Airways, with a major shopping center to the North and East, and mixed commercial on the South and West. The proposed site lies on Lamar which has a Northwest-Southeast orientation. This site will serve light rail users within a one mile radius of the site. Within this radius, the majority of land use is commercial with residential uses at the North, East, and South fringes, industrial use in the northwest fringe, and institutional uses in the southwest fringe. The service area is bounded on all sides with primarily residential use, with commercial uses in the Northwest and Southeast along Lamar.

Transportation use in this area is accommodated for automobile use, with an abundance of curb cuts and high amounts of access for vehicles at the expense of pedestrians. Within the service area, there is a lack of mixed housing and is predominated by low-income households. Near the site, the area is primarily commercial, with deteriorating structures and an emphasis on the shopping center to the North and East of the site, leads to architectural inconsistency among buildings. Within the site-lines along Lamar are a large billboard to the Southeast and the case building to the Northwest

The site's neighboring commercial uses contain an abundance of impervious surfaces that have a potential to contaminate groundwater with runoff. There also exists a lack of natural vegetation, and what is there does not provide for noise buffers along Lamar. Within the major commercial center along Lamar and on park there are auto-repair and salvage yards that may also present a pollution risk. There is also a lack of trees that would provide shielding from the sun. There is an overabundance of litter within the area that could be eliminated or lessened with the adoption of a neighborhood recycling program. Though there is a major commercial center, there are no nearby employment areas that would benefit from the development of a site in this area.



Existing strip commercial development at Orange Mound site.



Another example of existing commercial at Orange Mound site



Existing commercial development at Orange Mound site located next to vacant

ORANGE MOUND -

CONCEPT PLAN

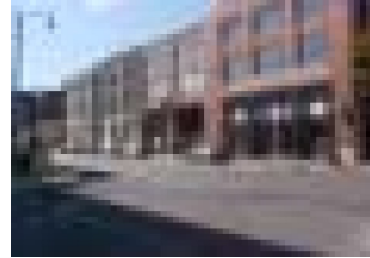
The light rail stop will feature a bus stop and multi-use interiors on the upper levels. Lower levels will provide an area to congregation of pedestrians and transit users. Upper levels will house office and meeting spaces to be utilized by community services and other organizations

Again, the sidewalks in this area will be six feet wide along Lamar Avenue, and connecting streets will be retrofitted to handle pedestrian capacity. Bike paths converge within the site that extends onto connecting streets to provide alternate routes of travel. Crosswalks have already been established in this area. Curb radii will be increased to 90° to decrease traffic speed at intersections, and improve pedestrian safety.

Along Lamar, a barrier will be established to improve pedestrian safety. The pedestrian realm will be properly proportioned along Lamar, with a ratio of 2:1 and built to human scale. Structures to be constructed (mixed use structures, parking garages, and multi-purpose transit stop) will be built to human scale, and maintain an architectural style that is representative of the architectural style of the area.

Ground texture in this area will be a combination of concrete sidewalks and brick surfaces. Brick surfaces are easy to maintain and repair. Ground texture will be given an aesthetically pleasing design that will enhance the pedestrian experience.

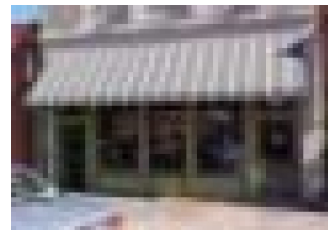
Signage within the area will be contiguous and not dominate or overwhelm existing architecture. Trash receptacles will be placed at street corners to alleviate littering. Also, lighting for the area will be adequate, but will not provide too much light that it creates a nuisance to residential areas.



Example of commercial facade from South Main, Memphis, TN



Example of commercial development, Montreal, Canada



Example of Commercial, South main, Memphis, TN.

ORANGE MOUND -

SITE PLAN

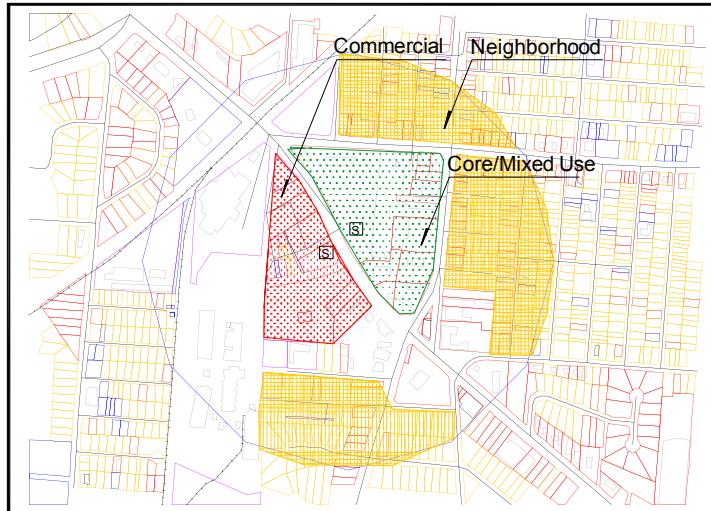
The overall site plan will have more variety in use than existing site. The Orange Mound site will have a primary use of Commercial with a core at the light rail stop. Creating a pleasant commercial environment will increase greater shopping traffic to orange mound neighborhood. Creating a beneficial area for an economically depressed community.

The site will consist of a reorganization of existing commercial, and a light rail transit stop at the core, with open space, creating an aesthetically pleasing environment.

Orange Mound

Kerr and Lamar Light Rail Stop

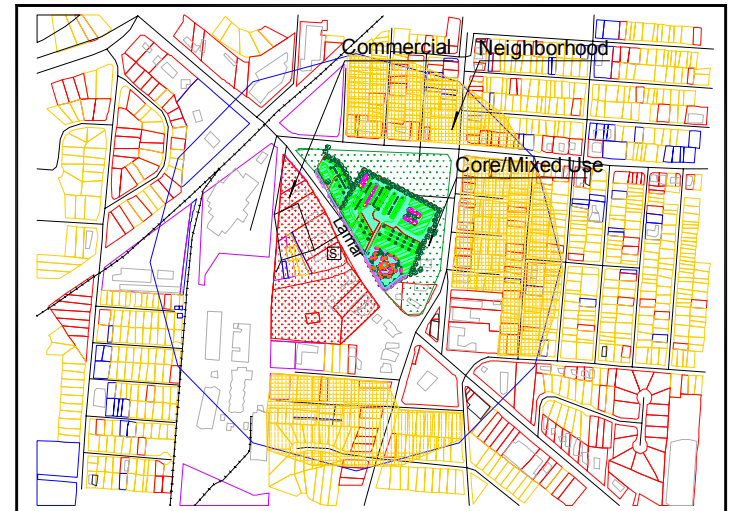
Existing Land Use



Existing:
Lamar Avenue currently lacks sufficient landscaping along the street.
The sidewalk along Lamar is broken in certain places and needs repair.
Pedestrian crosswalks are non-existing along Lamar Avenue.

Proposed:
In the proposed concept plan, Lamar will be heavily landscaped with repaired sidewalks and bicycle paths will be added. Lamar will also have one pedestrian crosswalk, located at the Kerr and Lamar intersection.

Proposed Land Use



Legend

Quarter-mile Buffer	
Roads	
Railroad	
Interstate	
Commercial	
Institutional	
Industrial	
Core/Mixed-Use	
Residential	
Footprints	



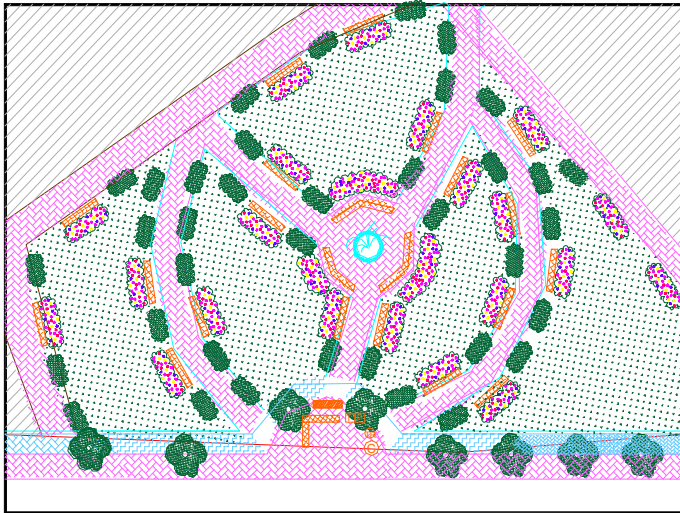
0 0.25 0.50 0.75 1.00 Miles

Site Planning Class Dec.2002

Orange Mound

Kerr and Lamar Light Rail Stop

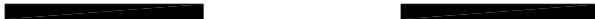
Open Space



Legend

Roads	—	Trees	
Footprints		Bushes	
Buildings		Flower Bed	
Pedestrian Path		Telephone	
Bicycle Path		Newspaper	
Grass		Trash Receptacle	
Bench		Clock Tower	
Fountain			

0



Core:

This area includes the proposed rail stop and current businesses such as the gas station and car wash. This core area will need to become more pedestrian friendly with landscaping and street lighting. There should also be retail shops within this core and common services for the residents of this community.



The core area of Lamar includes a bank, a gas station, and an vacant lot.

Neighborhood:

The neighborhoods within this community are not very pedestrian friendly. The sidewalks posses to many curb-cuts which make it difficult for pedestrians to travel along Lamar. The curbs are also to low and need to be raised.



Mixed - Use Area:



On the other side of Kerr there are a few buisnesses mixed in with various types of fast-food restaurants.

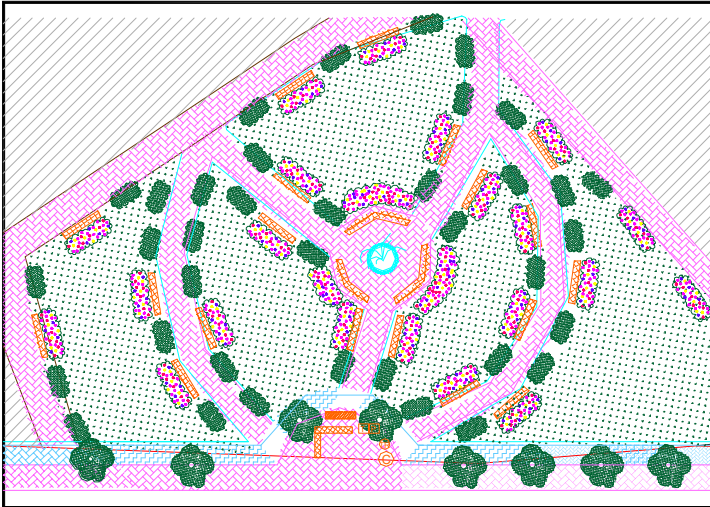
0.0625mile

Site Planning Class Dec.2002

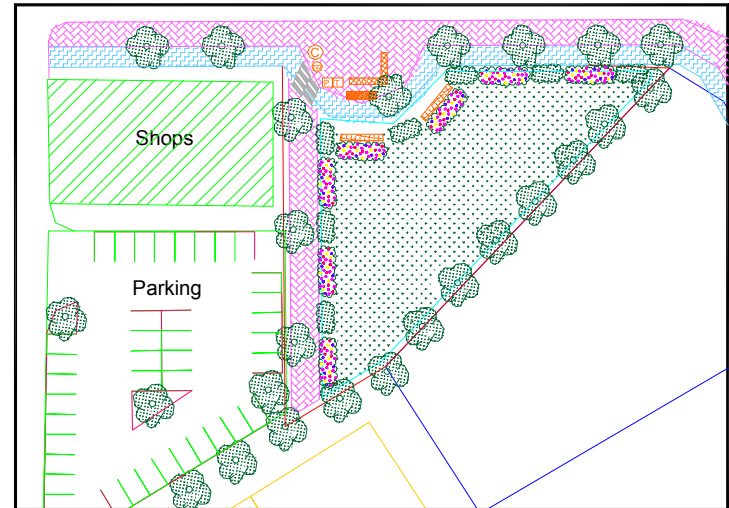
Orange Mound

Kerr and Lamar Light Rail Stop

Open Space 'A'



Open Space "B"



Example 1: Benches in the open space
Source:



Legend

Roads	—	Trees	
Footprints	▭	Bushes	
Buildings	▨	Flower Bed	
Pedestrian Path	▤	Telephone	
Bicycle Path	▥	Newspaper	
Grass	▦	Trash Receptacle	
Bench	▧	Clock Tower	
Fountain			

0



Open Space:

The proposed open space will provide a central meeting place for the residents who live in the surrounding neighborhood. The fountain will be the focal point of the plaza. Adequate walking paths and benches make it pedestrian friendly.

Example 2: Fountain in the open space
Source:

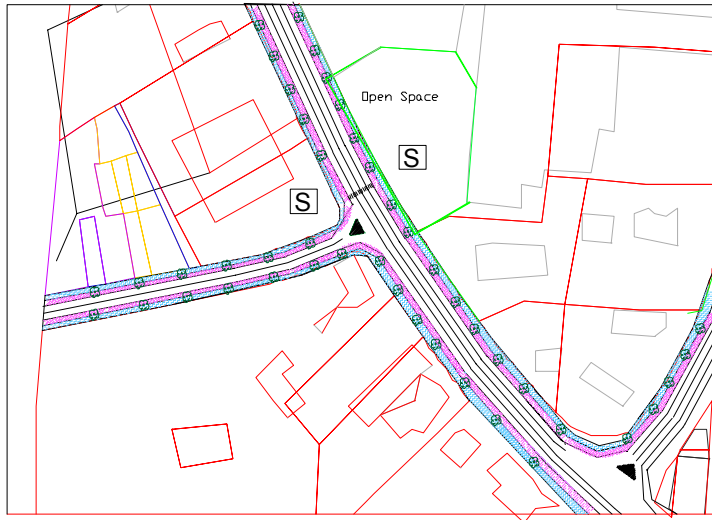


Site Planning Class Dec.2002

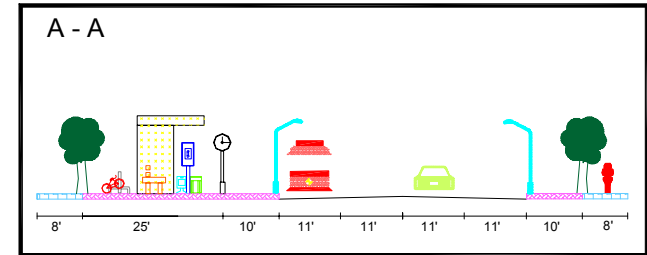
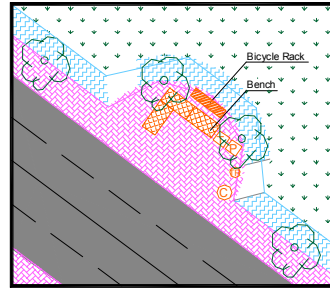
Orange Mound

Kerr and Lamar Light Rail Stop

Rail Stop Area



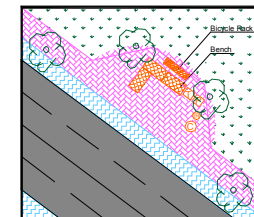
Rail Stop



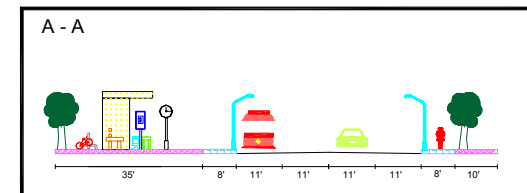
Rail Stop:

The proposed rail stop is at the corner of Lamar and Kerr. This stop will include benches with a canopy overhead and an information kiosk. A bicycle rack, trash receptacle, a newspaper stand, a telephone, and a clock are all amenities to the stop (see map inset). Included in this stop will be trees and bushes to create a buffer between the street and sidewalk. A bicycle path is also provided adjacent to the sidewalk.

Alternative 1 - Bicycle Path



Bicycle path will be adjacent to the street.



Legend

Roads	Mixed Use	
Railroad	Neighborhood	
Interstate	Pedestrian Path	
Commercial	Bicycle Path	
Institutional	Transit Stop	
Industrial	Telephone	
Open space	Newspaper	
Residential	Trash Receptacle	
Footprints	Clock Tower	



0.25 mile

Site Planning Class Dec.2002

Lamar Avenue Transportation Oriented Development District Zoning District Requirements

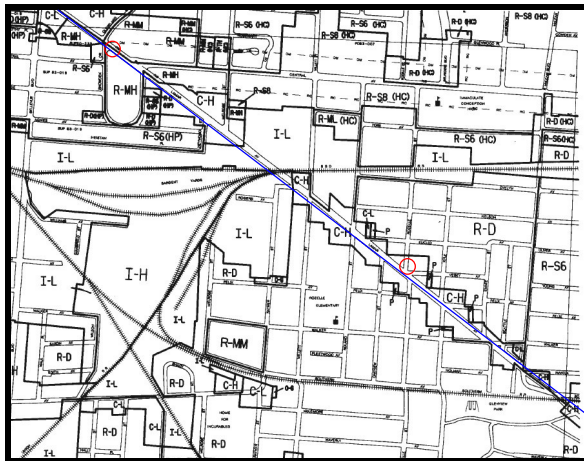
Current Boundaries and Definition of the District: The Lamar Avenue TOD District includes the section of Lamar Avenue located between Bellevue and Trezevant and consists of four sub areas. The four sub areas are each centered on a proposed light rail stop and include all of the surrounding area within a quarter mile radius. The proposed light rail stops are located at the intersection of Lamar Avenue with Cleveland, Rozelle, Carnes, and Kerr. The Cleveland, Rozelle, and Carnes zones are designated as neighborhood core areas. The Kerr zone is designated as a regional core area.

Current Zoning: The current dominant type of commercial zoning along Lamar Avenue TOD development district is Highway Commercial (C-H). This zoning district is designed to permit the development of general commercial uses in a linear spatial form along highways, industrial areas, and transportation facilities. This district does not allow typical residential uses such as single family detached, two family, townhouse, or multiple family. Hotels, group homes, transitional shelters, and accessory dwelling units are allowed. All commercial uses are allowed with warehouses, communication towers and vehicle washes requiring a site plan review.

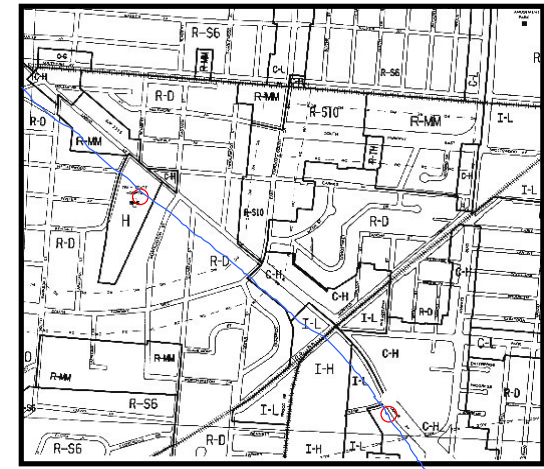
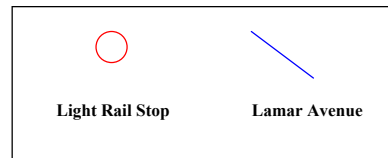
The proposed district also contains areas of light industrial district zoning between Cleveland and Rozelle and in the Lamar Kerr area. The intent of the light industrial district (I-L) is to allow manufacturing, wholesaling, or warehousing that is located near major transportation routes and that will have minimal impact upon surrounding districts. The Kerr site also contains heavy industrial district zoning (I-H) in which all industrial uses are allowed.

Residential district zoning includes duplex residential zoning (R-D) which is intended to permit single family and two-family dwellings. Two multiple dwelling residential districts, (R-MM) and (R-MH), are located along the Lamar Avenue Corridor. R-MM is designed to permit single family, two family, and multiple family dwellings. R-MM is designed to permit the development of all housing types except mobile homes in areas near major transportation routes and employment centers. Several small pockets of single family residential zoning exist along the Lamar Avenue Corridor, including R-S6, R-S8, and R-S10 requiring minimum lot sizes of 6,000, 8,000, and 10,000 square feet respectively. Two special purpose districts, hospital (H) and parking (P) are located within the TOD district. The hospital district is located at the intersection of Carnes and Lamar. This district allows single family and two family uses as well as parks, group shelters, and transitional homes. Multiple family, townhouse, office and retail uses require site plan approval.

Maps of Current Zoning Districts in Lamar Avenue TOD District



Cleveland/Rozelle



Carnes/Kerr

Existing Bulk Regulation and Density Requirements:

Bulk regulations and Density	Lot Area Square Feet	Lot Width (feet)	Front setback	Side Setback	Rear Setback	Maximum Height (Feet)	Maximum Gross Density (per acre)	Maximum FAR
C-H District	None	None						
1. all uses except offices			30	10	15	50	None	None
2. offices			30	10	15	50		3.0
I-L District	None	60	30	10	15	100	None	None
I-H District	None	60	30	10	15	100		
R-D District								None
1. Single family detached	6,000	50	30	5	25	35	7.3	
2. Two family	6,000	50	30	5	25	35	14.6	
3. Single Family attached	3,000	25	30	5	25	35	14.6	
R-MM								
1. Single Family Detached	5,000	50	30	5	25	35	8.7	None
2. Townhouse	2,500	16	30	5	25	35	15	.28
3. Two family	5,500	50	30	5	25	35	15.8	None
4. Single family attached	2,750	25	30	5	25	35	15.8	None
5. Multiple family	10,000	100	30	5	25	None	30	1.75
R-MH								
1. Single Family Detached	5,000	50	30	5	25	35	8.7	None
2. Townhouse	2,400	16	30	5	25	35	18.2	.28
3. Two family	4,800	50	30	5	25	35	18.2	None
4. Single family attached	2,400	25	30	5	25	35	18.2	None
5. Multiple family	10,000	100	30	5	20	None	75	1.75

*R-S6, R-S8, and R-S10 setback requirements are the same as listed for single family detached in R-MM.

Purpose and Intent of the District: The purpose of the Lamar Avenue Transportation Oriented Development District is to utilize the development of a light rail route on Lamar Avenue as a catalyst for economic development and physical improvement of the area.

Specifically the district seeks to accomplish the following goals:

- To encourage the development of mixed-use core areas adjacent to the light rail stops that will include public open space, high density residential, commercial, and office uses.
- To provide increased opportunities for, and access to employment and affordable housing.
- To improve the physical environment of the area through the provision of public open space, landscaping, and defined pedestrian and bicycle routes.
- To encourage and develop the linkage of the proposed light rail route on Lamar Avenue to existing public transportation routes.
- To promote the development and enhancement of streets and buildings on a human scale.
- To develop a formalized planning area that will encourage the participation of local community residents and organizations.
- To preserve the quality of existing residential areas and improve residential areas which are in a state of decay.

Neighborhood Core

Permitted Uses (Residential):

Single Family Detached Dwellings
Single Family Attached Dwellings
Duplex Dwellings
Townhouse Dwellings
Multiple Family Dwellings
Accessory Dwelling Unit

Additional Uses Permitted:

Mixed Use Residential/Retail/Office
Retail Sales and Shops
Church
Daycare Center
Park
Public Building
School (Public or Private)
Art Gallery, Bakery (retail), Bank, Beauty shop, Cleaning Establishment, Financial Services,
Flower or Plant Store, Offices, Restaurant.

Urban Core

Permitted Uses (Residential):

Single Family Detached Dwellings
Single Family Attached Dwellings
Duplex Dwellings
Townhouse Dwellings
Multiple Family Dwellings
Accessory Dwelling Unit

Additional Uses Permitted:

Retail Sales and Shops
Church
Daycare Center
Park
Public Building
School (Public or Private)
Art Gallery, Bakery (retail), Bank, Beauty shop, Cleaning Establishment, Financial Services,
Flower or Plant Store, Offices, Restaurant, Gasoline Service Station, Radio or TV, Parking
Garage (first floor should provide 75 percent retail).

Neighborhood

Commercial Core



(1)



Dense Mixed Use Commercial Node (2)

Residential Core



Multifamily (3)



Townhouse (4)

Urban

Commercial Core



Mixed Use Loft (5)



Parking/Retail (6)

Residential Core



Mixed Use (7)



3 Story Apartments (8)

Transition

Neighborhood



Single Family Residential (9)



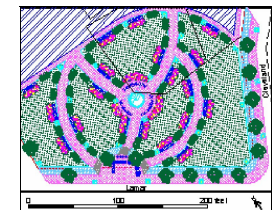
Public School (10)



Public Building (11)



Neighborhood Park (12)



Open space (13)

Additional Requirements:

A drive through window shall not be permitted in neighborhood core areas. Neighborhood core areas are intended to emphasize pedestrian orientation and human scale. Drive through windows enforce reliance upon the automobile and would be a contradiction to the intent of this area. All new construction shall be subject to site plan review in order guide the development process in a fair and logical manner.

Sidewalks and Bicycle paths: A sidewalk network shall be provided and maintained throughout the district that connects residential, non residential and open space areas. Sidewalks shall be separate and distinct from vehicle areas and shall include street furniture and landscaping. Sidewalks shall be a minimum of four feet in width with recommended width of six feet along major pedestrian routes, and ten feet in commercial areas. Pedestrian street crossings shall be clearly defined by a change in pavement color and/or texture.

Bicycle paths shall be provided where appropriate to link transit core areas with peripheral areas. Clearly defined and separate bicycle paths shall be provided on streets with a high daily average traffic count. Bicycle racks shall be provided in the central core (station) area and in public open space areas.

Parking Requirements:

Parking shall be located at the side of a building and not between the building and the street. Parking lots shall be landscaped and designed to avoid the infiltration of noise and light pollution into adjacent (especially residential) areas.

The interior of all parking lots shall be landscaped to provide shade.

Required parking:

- Residential- one space per unit
- Retail - one space per each 1,000 square feet of floor area.
- Office - one space per each 600 square feet of floor area.
- Public/Institutional - one space per 500 square feet of floor area.

Signs:

Neighborhood Core- All signs shall be attached to a building. Detached signs are not permitted. Signs should reflect the scale and design of the building. All signs shall have a maximum of 35 square feet on each side. Current zoning allows detached (75 square feet) and portable signs.

Urban Core- One detached sign per 200 feet of road frontage. The design of signs shall be ground mounted monument style with a maximum height of ten feet and maximum square footage of 60 square feet. One attached sign per establishment is permitted. Portable wheel type signs are not permitted. Current zoning allows detached (75 square feet) and portable signs.



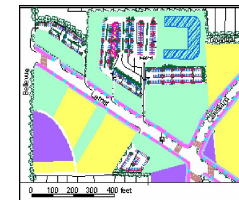
Sign (14)



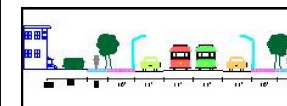
Neighborhood Sign (15)



Bicycle path (16)



Parking and Landscaping (17)



Street design, Setbacks, and Landscaping (18)

Proposed Bulk Regulation and Density Requirements

Bulk Regulations and Density	Lot area square feet	Width (feet)	Front setback	Side setback	Rear setback	Height (stories)	Net density (per acre)	Floor Area Ratio
Neighborhood Core Commercial and Mixed Use	Minimum: 2,500 Maximum: 16,000	Minimum: 20 Maximum: 50	Minimum: 0 Maximum: 10	Minimum: 0 Maximum: 20	Minimum: 15 Maximum: 75	Minimum: 2 Maximum: 5	none	Minimum: 0.30 Maximum: none
Urban Core Commercial and Mixed Use	None	Minimum: 20 Maximum: 80	Minimum: 0 Maximum: 60	Minimum: 0 Maximum: 20	Minimum: 55 Maximum: 110	Minimum: 3 Maximum: 8	None	Minimum: 0.30 Maximum: none
Single Family Detached Residential	Minimum: 5,000 Maximum: 10,000	Minimum: 50 Maximum: 65	Minimum: 10 Maximum: 25	Minimum: 5 Maximum: 20	Minimum: 25 Maximum: na	Minimum: 1 Maximum: 2	Minimum: 4 Maximum: 8	None
Single Family Attached Residential	Minimum: 3,000 Maximum: 5,000	Minimum: 40 Maximum: 80	Minimum: 10 Maximum: 25	Minimum: 5 Maximum: 20	Minimum: 25 Maximum: na	Minimum: 1 Maximum: 2	Minimum: 8 Maximum: 14	None
Two Family Residential	Minimum: 3,000 Maximum: 5,000	Minimum: 30 Maximum: 50	Minimum: 10 Maximum: 20	Minimum: 4 Maximum: 10	Minimum: 25 Maximum: na	Minimum: 1 Maximum: 2	Minimum: 8 Maximum: 14	None
Townhouse	Minimum: 1,800 Maximum: 4,500	Minimum: 16 Maximum: 30	Minimum: 0 Maximum: 15	Minimum: 5 Maximum: 12	Minimum: 20 Maximum: 30	Minimum: 2 Maximum: 3	Minimum: 18 Maximum: 29	Minimum: 0.30 Maximum: 0.60
Multi Family Residential	Minimum: 8,000 Maximum: 15,000	Minimum: 80 Maximum: 120	Minimum: 0 Maximum: 20	Minimum: 5 Maximum: 20	Minimum: 25 Maximum: 50	Minimum: 3 Maximum: 5	Minimum: 30 Maximum: 60	Minimum: 0.40 Maximum: 0.60

Bulk Regulation and Density Requirements are modified from current standards in order to incorporate the design elements of transportation-oriented development into the redevelopment of the Lamar Avenue Light Rail Corridor. Setbacks have been reduced in order to create a pleasant streetscape designed on a human scale and oriented towards pedestrians. Minimum and maximum density and floor area ratios are established with the goal of increased density and intensity of land use.

Maintenance:

Although the city of Memphis shall have primary responsibility for providing public services such as trash pickup, street cleaning, and maintenance of public infrastructure, local community organizations shall be encouraged to assist with landscaping projects and the identification neglected property.

Appropriate lighting shall be provided in all public areas to promote and insure a safe public realm.

Property that falls into disrepair shall be subject to the following:

Inspection.

Warning, with defined time frame for repair.

Legal action including but not limited to property lines and withholding of occupancy permits.

Sources:

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Images:

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