



# LAMAR CORRIDOR TRANSIT ORIENTED REVITALIZATION PLAN

MEMPHIS, TENNESSEE

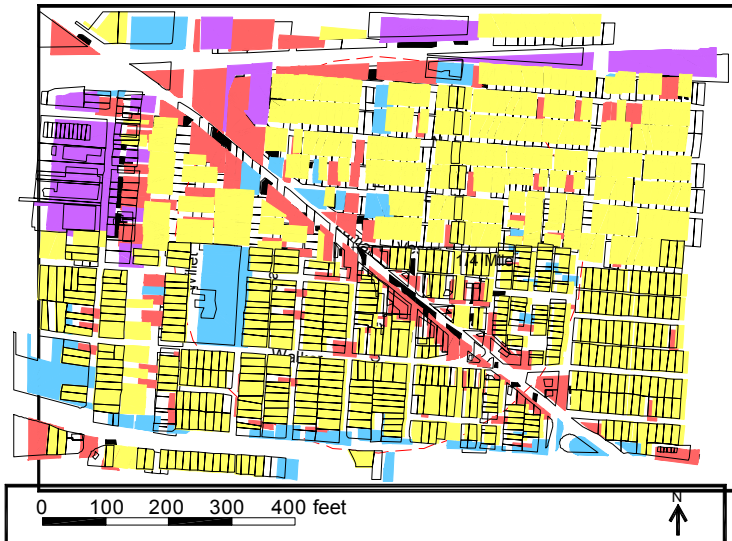
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PREPARED BY:  
PLAN 7008: SITE PLANNING  
FALL 2002

# Glenview

## Lamar Rozelle Light Rail Stop

Existing Land Use



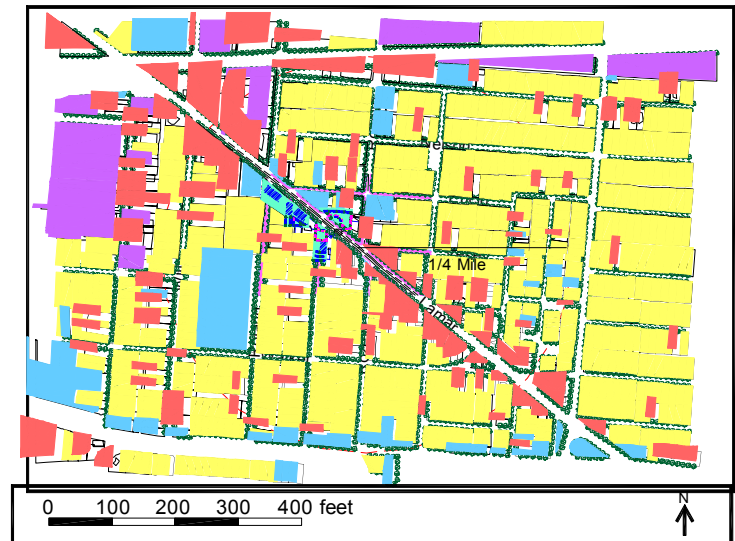
Existing:

Lamar Ave. currently has very little landscaping and the sidewalks are broken and insufficiently wide. There are no crosswalks or other access across Lamar. There is too much space in between buildings; many are vacant and dilapidated.

Proposed:

This plan envisions heavy landscaping along Lamar with sidewalk widening and repair. A crosswalk will be added at the intersection of Lamar and Rozelle, the proposed site for the light rail stop. Building density should be high.

Proposed Plan



Quater-mile Buffer  
Commercial  
Institutional  
Industrial  
Mixed-Use  
Residential  
Existing Buildings

Buffer

Transit Stop

Proposed Commercial/Office

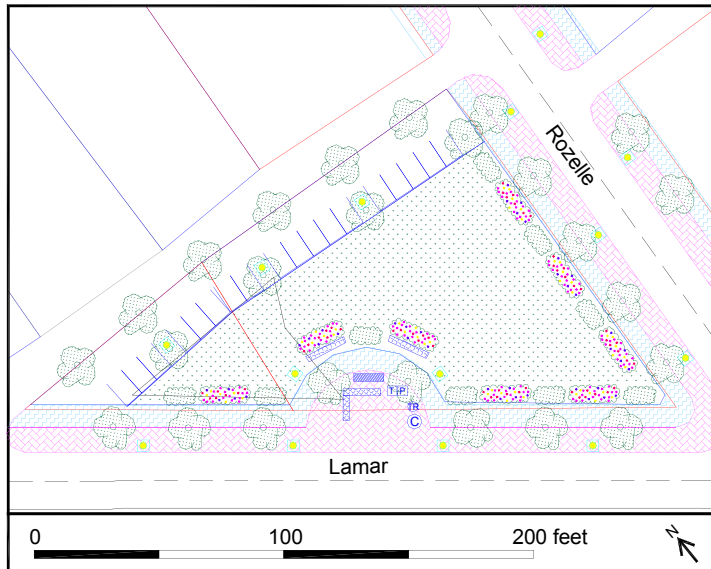
Landscaping

S

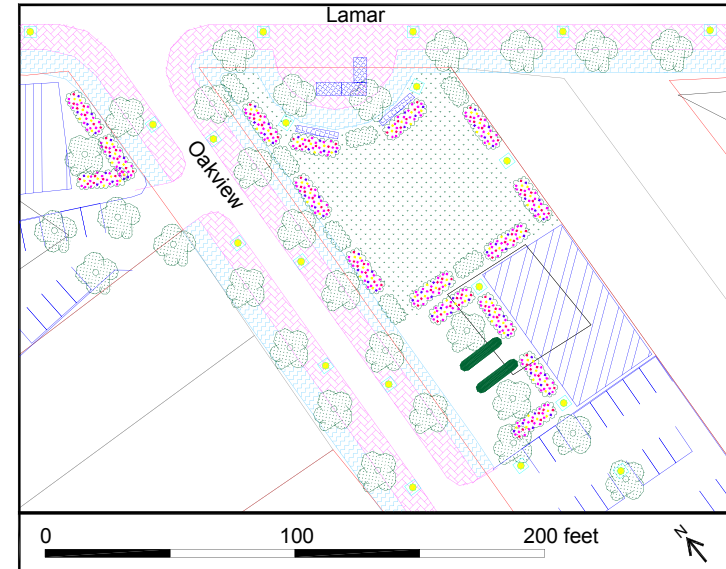
# Glenview

## Lamar and Rozelle Light Rail Stop

Open Space "A"



Open Space "B"



- |                            |  |                  |  |
|----------------------------|--|------------------|--|
| Proposed Commercial/Office |  | Telephone        |  |
| Pedestrian Path            |  | Newspaper        |  |
| Bicycle Path               |  | Trash Receptacle |  |
| Grass                      |  | Clock Tower      |  |
| Bench                      |  | Street Lights    |  |
| Landscaping                |  |                  |  |
|                            |  |                  |  |

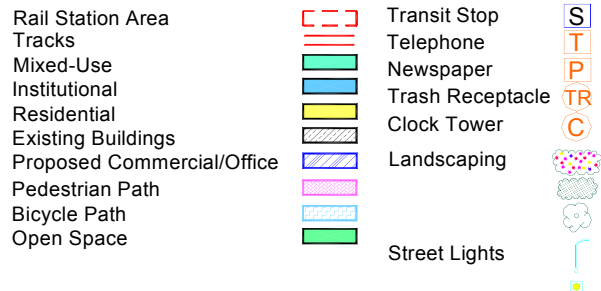
*On both sides of Lamar from the light rail stop there will be small park-like open space. Landscaping, trees and flower beds, will form the border for the open space. Immediately surrounding the open space will be parking for light rail passengers as well as small retail shops and restaurants.*

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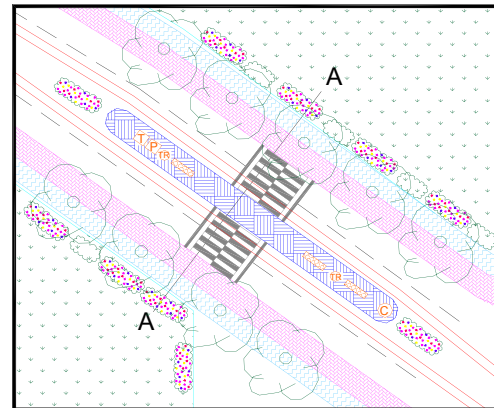
# Glenview

## Lamar and Rozelle Light Rail Stop- Center Alignment

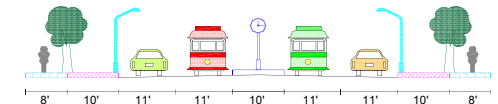
Alternative 1 - Rail Station Area



Rail Station

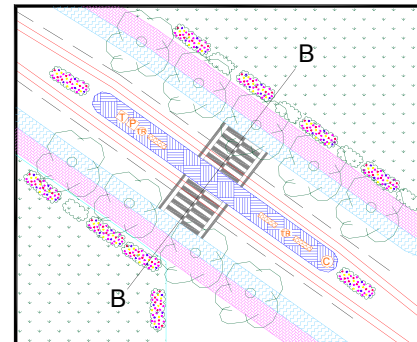


A - A Lamar Cross section

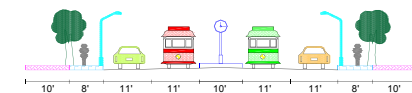


*Bicycle path will be next to the sidewalk.*

Alternative 1 - Bicycle Path



B - B Lamar Cross Section Alternative



*Bicycle path will be next to the street.*

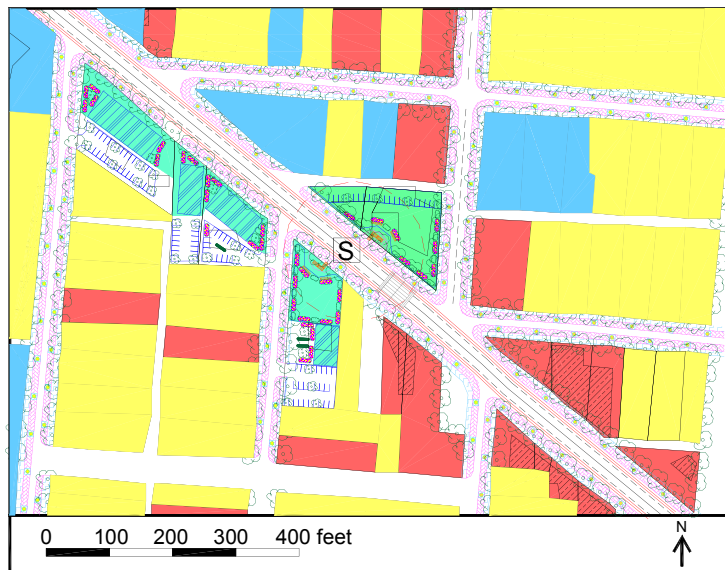
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## Lamar and Rozelle Light Rail Stop- Side Alignment

Rail Station Area

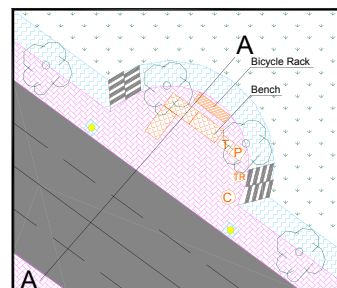


Rail Station Area  
Tracks  
Mixed-Use  
Institutional  
Residential  
Existing Buildings  
Proposed Commercial/Office  
Pedestrian Path  
Bicycle Path  
Open Space

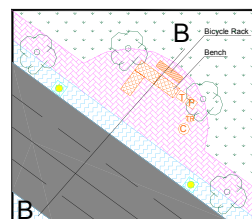
Transit Stop  
Telephone  
Newspaper  
Trash Receptacle  
Clock Tower  
Landscaping  
Street Lights

S  
T  
P  
TR  
C

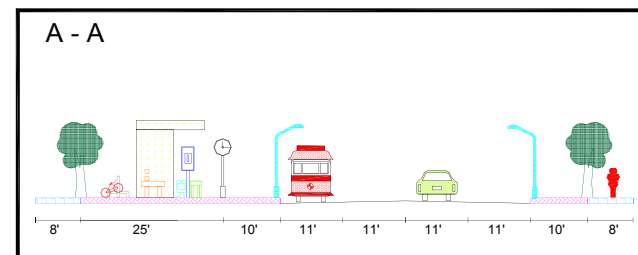
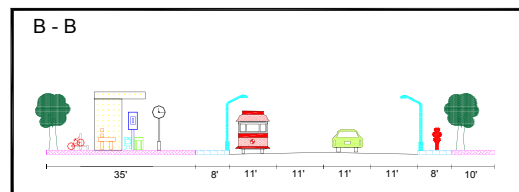
Rail Station



Alternative 1 - Bicycle Path



*Bicycle path will be next to the street.*



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# COOPER-PARKWAY -

## EXISTING CONDITIONS

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Currently, the area to be preserved by the proposed site does not provide adequate services, facilities, and jobs within walking distance. Within the proposed core, there is a small amount of commercial use and very little pedestrian traffic. Sidewalks in this area are narrow along Lamar and are in disrepair along Lamar and connecting streets. No crosswalks exist or no ease in pedestrian cross traffic on busy Lamar. The core does not lend itself to pedestrian safety and curbs do not provide ramps to assist disabled pedestrians.

Lighting in this area is adequate, but requires improvement by adding more light sources to improve pedestrian safety. This area also does not contain a bike path for safe travel for cycles.

The core of this site does not have any public spaces either for recreational use or for any type of community commons area. The surrounding neighborhood is high density residential and there is one park at the south end of the site's radius.

The trolley stop development at the intersection of Carnes and Lamar will include a mixed use center of first floor commercial uses with multi-family housing located on upper floors of structures. This will create a high net density of uses within the site's core development.

Development within this area is focused on creating a plaza environment built to human scale to accommodate pedestrian congregation and exchange while also providing as an area for access to transit located at the core, surrounded by mixed use commercial and residential to the northwest and west of the light rail stops. The proposed commercial areas are located along Carnes and Castalia, and will reflect architectural style along this area.



*Dilapidated and deteriorating housing directly to the north of the Cooper-Parkway Node*



*Existing commercial use at Cooper-parkway Node.*



*Institutional use located at Cooper-Parkway Node.*

# COOPER-PARKWAY - CONCEPT PLAN

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With the creation of the light rail transit, there is expected increase of pedestrian traffic along the Lamar corridor, and connecting streets. Sidewalks along Lamar should be increased to accommodate increased pedestrian traffic. As it stands, there is no uniform sidewalk width along Lamar Avenue. At the Carnes site, sidewalk length varies from 3 feet to six feet, 5 inches. It is necessary to create a consistent pedestrian path at a width of six feet to accommodate expected pedestrian traffic increase along Lamar, and expand and build sidewalks along connecting streets that will accommodate increased pedestrian traffic along this area. The sidewalks along Lamar will be placed behind a barrier from the street that will enhance pedestrian safety and sense of security.

Crosswalks will be placed at the corners of Carnes and Lamar and at the intersection of Lamar, Castalia, and Worthington along with necessary streetlights that will provide for the ease of pedestrian cross traffic. Sidewalks and curb ramps will be constructed to provide for access to persons with disabilities. Curb Radii at all corners will be changed to 90° angles that will create shorter crossing distances and slow traffic for pedestrian safety.

The plaza area will consist of a mixed texture of concrete and brick surface with interspersed green areas that will create an environment that is aesthetically pleasing and will increase pedestrian and transit usage through the area.

The pedestrian realm will be properly proportioned at a scale of 2:1 along Lamar and Worthington. Along Carnes, there is an approximately six-foot embankment that will affect human proportion and will be landscaped to create a terrace effect that does not overwhelm or isolate the pedestrian from the commercial uses on the property.

Lighting and signage within the commercial core will be enhanced to create a realm of security for the pedestrian. Lighting will be adequate to light sidewalks and streets, but not to the extent that it creates a source of light pollution that will disturb neighboring residential areas. Signage for retail areas and for rail stops will not dominate or detract from the architectural style of the area. Trash receptacles will be placed at the intersections of the main core areas to prevent and discourage littering.

Bicycle paths will be established along Lamar and connecting streets that provide another alternative to pedestrian or vehicular traffic. Bicycle paths will be placed furthest from the street on opposite side of sidewalks. Bike paths will be six feet to accommodate two-lane bicycle traffic.

The plaza areas at Carnes will be constructed with park benches and areas that will provide for passive recreation, allowing for the congregation of pedestrians and transit users. These benches and areas will also provide areas for transit users to wait for the train. The plazas will be constructed with small green areas that will create a more aesthetically pleasing environment for the pedestrians and transit users. Sidewalks and other open areas located away from the commercial and transit center will be dedicated to the local homeowners association, who will maintain these areas. The plaza areas will be dedicated to the city, which will be responsible up the upkeep and maintenance of the plazas.

The streetscape at Lamar and Carnes conforms to a number of important principles in developing a positive community image, but is lacking in a few of the attributes of a positive streetscape. The streets are laid out hierarchically, with Lamar as the major arterial and Carnes and other intersecting streets serving as collectors. The network of streets and land use give a sense of direction and enclosure for both pedestrians and vehicles. The proportion of the streetscapes is large with the width far larger than the height of the buildings, however, there is substantially landscaping with large oak trees which provide a more balanced proportion. The light rail plaza and stop should have street furniture such as benches, trash cans, and street lights to increase the comfort, cleanliness, and safety of the rail stop and surrounding commercial buildings. The new commercial buildings should be proportioned to give a feeling of enclosure. The setback line should be close to the sidewalk to give a pedestrian friendly feel. Entrances on to sidewalks should be large and inviting. Parking for this new development should be moved to the back and sides of the building.



*This building in Missoula, MN is an example of a mixed use building*



*This development in Pheonix, AZ is an example of mixed-use housing.*

# COOPER-PARKWAY -

## SITE PLAN

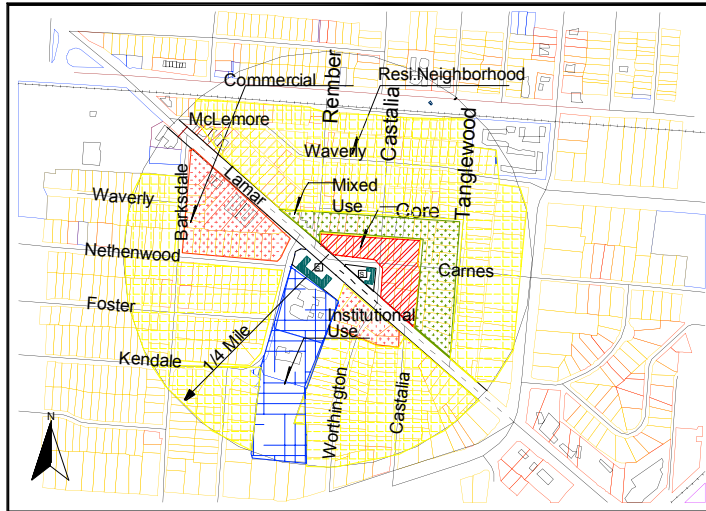
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The overall site plan will have more variety in use than the Carnes site. The Carnes site will have a primary use of residential with a commercial core at the light rail stop. There are elements of mix within this site, however. A variety in the residential housing provides for a mix of household incomes. Since this area is one of the most affluent in the site area, yet still below the median income for the Memphis MSA, it is not feasible or desirable to attempt to mix the income of this area to an even greater extent. However, adding residential apartments to the top level of the new commercial center, will add a greater mix of demographic characteristics to the neighborhood, including more singles and elderly residents.



# Cooper/Parkway

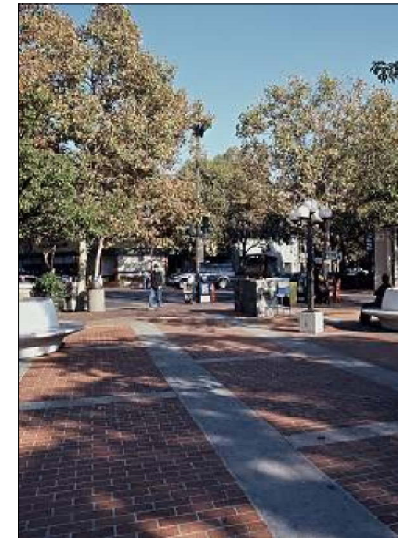
## Concept Plan for Carnes and Lamar Light Rail Site



### Legend

Quarter-mile Buffer	
Roads	
Railroad	
Interstate	
Commercial	
Institutional	
Industrial	
Mixed-Use	
Residential	
Footprints	
Core	
New Footprints	

- **Inner Core:**  
*This area consists of light rail stop and open space for pedestrians and light rail users.*
- **Outer Core/Mixed Use:**  
*Outer Core will sport a mix of commercial retail and office space with high density residential.*
- **Institutional Use:**  
*This area consists of a large institutional use and the other light rail stop is located in this area.*
- **Residential neighborhood:**  
*This area is occupied by high density residential units.*
- **Commercial Use:**  
*This area is predominated by commercial activity.*



Example of Plaza like environment in Boston, MA

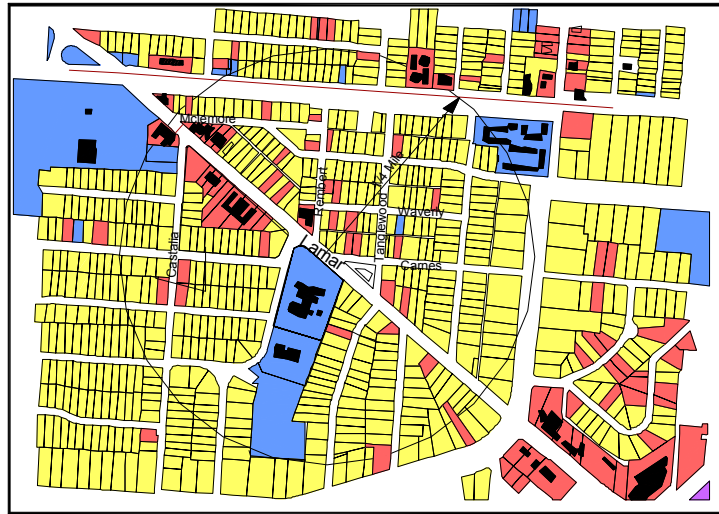









Example of mixed-use building in Pheonix, AZ

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# Cooper/Parkway

## Existing Land Use



Quater-mile Buffer   
 Railroad   
 Interstate   
 Commercial   
 Institutional   
 Industrial   
 Residential   
 Existing Buildings 



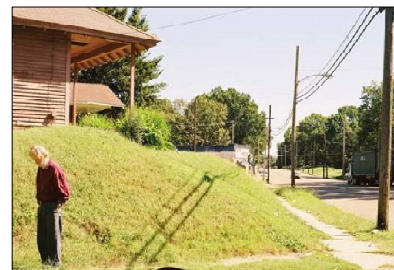
Institutional  
Use/OpenSpace

### Existing Conditions:

- *Inconsistent sidewalks*
- *Limited pedestrian access*
- *No bicycle path*
- *No mix of Land Uses*
- *Virtually no streetscape, Land Scape*



Dilapidated residential  
area

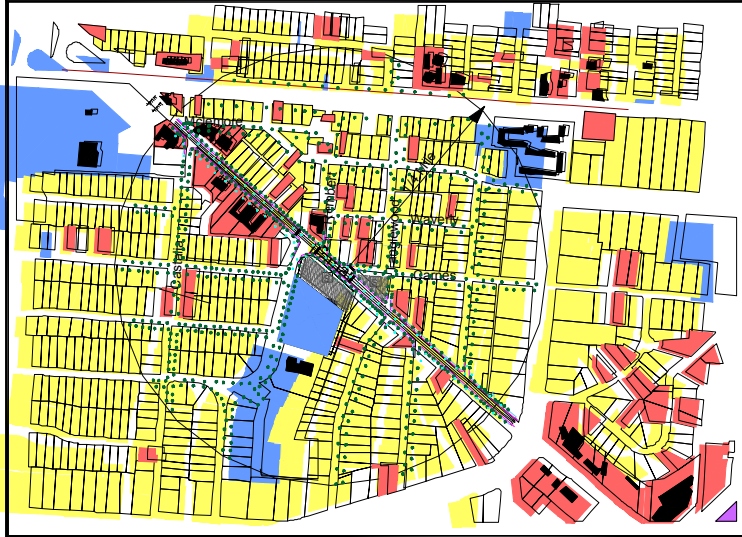


Vacant housing

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# Cooper/Parkway

## Proposed Land Use



Quater-mile Buffer		Transit Stop	
Railroad		Proposed site area for stops	
Interstate		Landscaping	
Commercial			
Institutional			
Industrial			
Residential			
Existing Buildings		Street Lights	



Quality Residential area

Proposed:

- *consistent sidewalks*
- *Added bicycle path*
- *Attractive landscape*
- *Sufficient lighting*
- *Crosswalks*

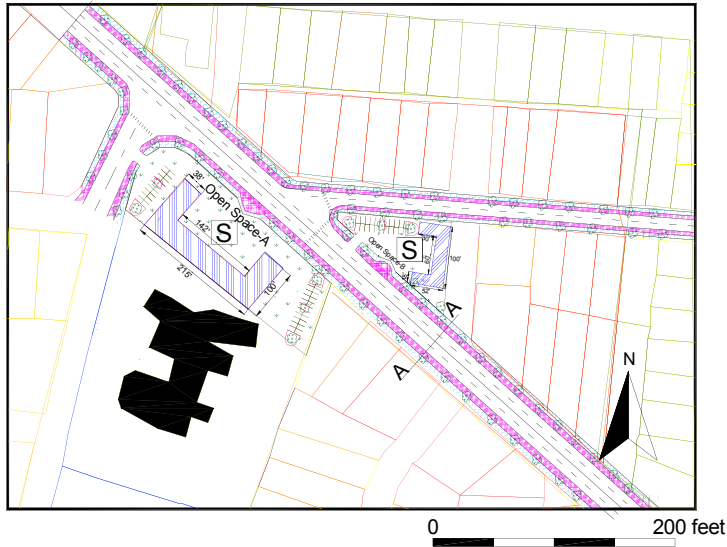


Example of Streetscape from portland, OR

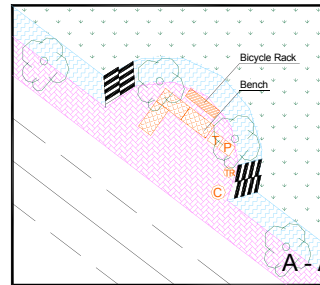
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# Cooper/Parkway

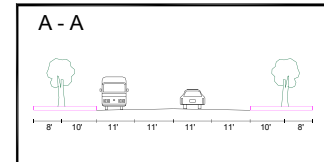
## Lamar and Carnes Light Rail Stop



- |                     |                   |                  |                     |
|---------------------|-------------------|------------------|---------------------|
| Commercial          | [Red outline]     | New FootPrints   | [Blue hatched box]  |
| Institutional       | [Blue outline]    | Bicycle Path     | [Blue dashed box]   |
| Industrial          | [Purple outline]  | Neighborhood     | [Yellow dashed box] |
| Mixed-Use           | [Green outline]   | Pedestrian Path  | [Pink dashed box]   |
| Residential         | [Orange outline]  | Clock Tower      | [Orange dashed box] |
| Existing Footprints | [Black solid box] | Transit Stop     | [C in circle]       |
|                     |                   | Telephone        | [S in circle]       |
|                     |                   | Newspaper        | [T in circle]       |
|                     |                   | Trash Receptacle | [P in circle]       |
|                     |                   |                  | [TR in circle]      |

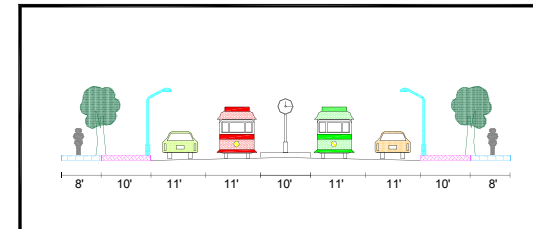


Transit Stop



View of Section A-A

Alternative of Section A-A

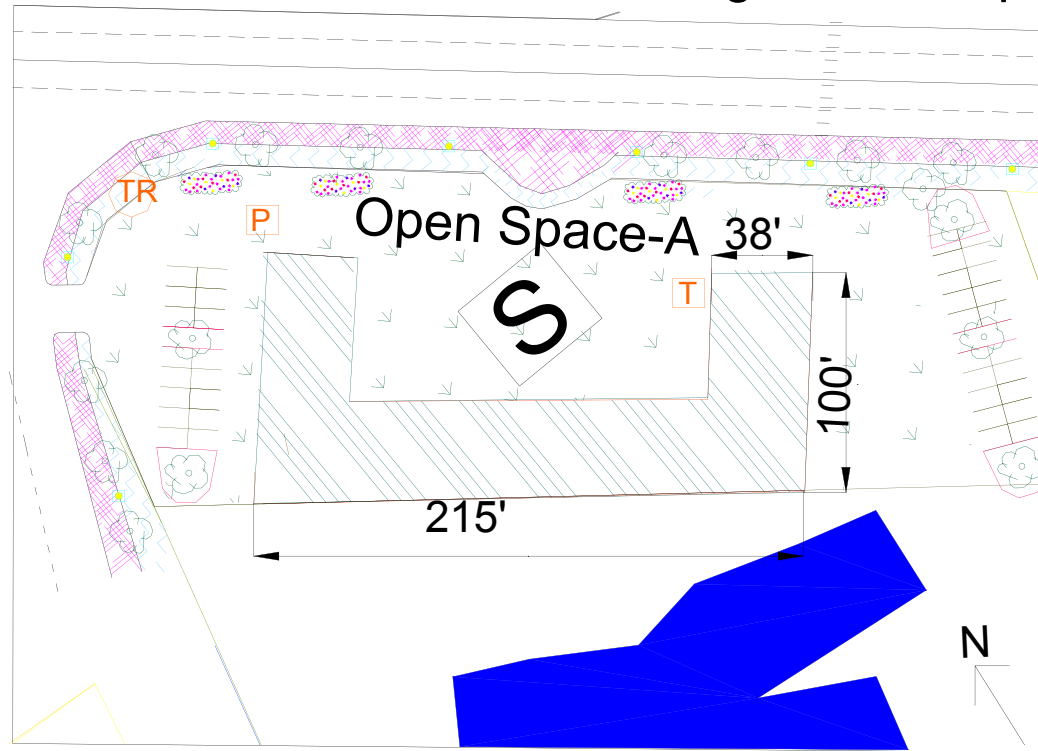


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# Cooper/Parkway

## Lamar and Carnes Light Rail Stops with openspace



- |                   |  |
|-------------------|--|
| Road              |  |
| Transit Stop      |  |
| Institutional use |  |
| Telephone         |  |
| Newspaper         |  |
| Trash Receptacle  |  |
| New footprints    |  |
| Bicycle Path      |  |
| Pedestrian Path   |  |
| Land Scaping      |  |
| Street Lights     |  |

Open Space-A

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